

# DEVELOPMENT PLAN SUBMITTAL

# BUSHNELL FARM

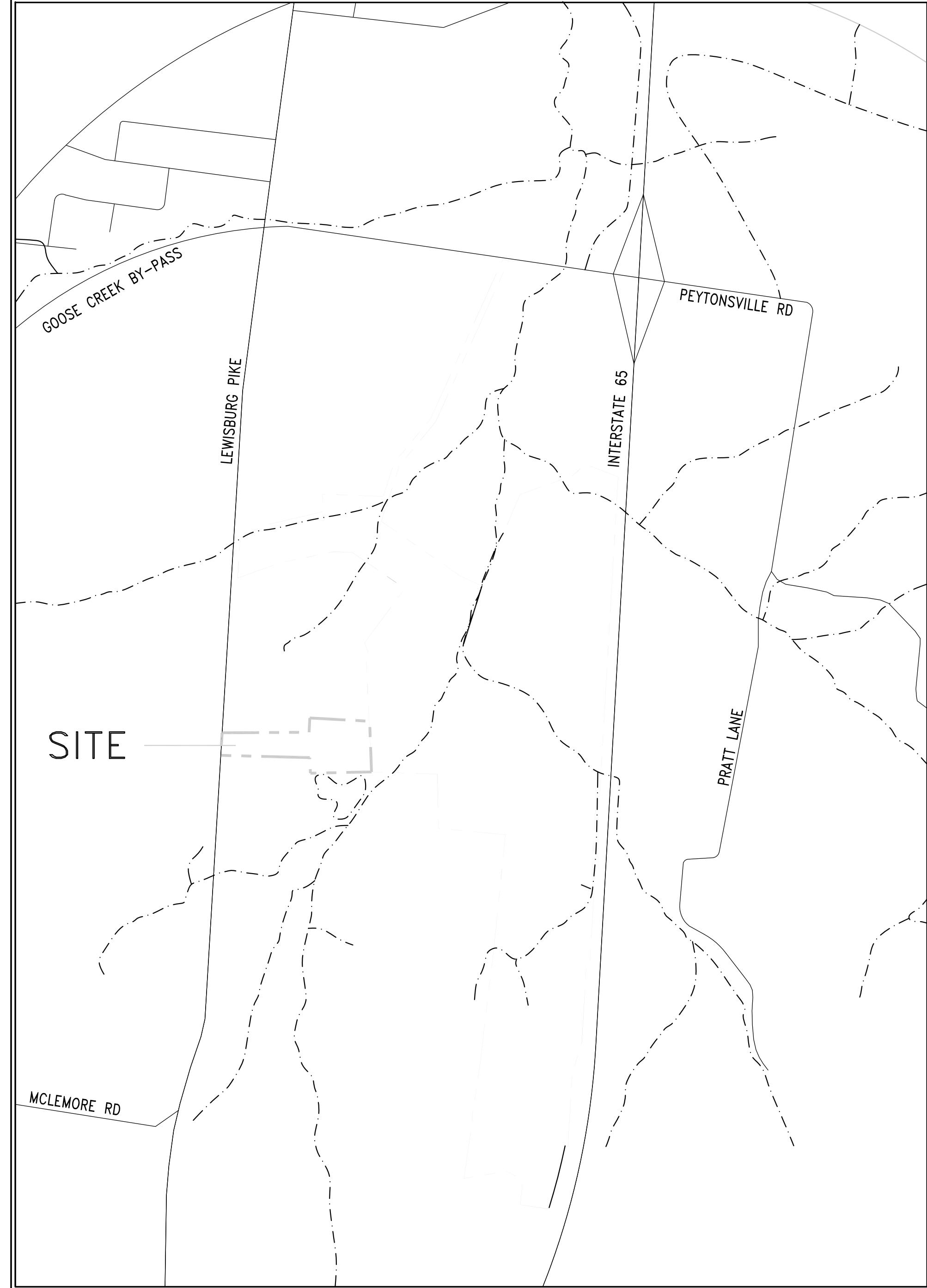
## CONSERVATION SUBDIVISION

### ZONING: SD-R

DENSITY: 2.90 Du/Ac

**FRANKLIN, TENNESSEE**  
**CITY OF FRANKLIN PROJECT NO. 6471**

PRE-APPLICATION DEVELOPMENT PLAN SUBMITTAL: MAY 1, 2017  
 DEVELOPMENT PLAN SUBMITTAL: JUNE 12, 2017  
 DEVELOPMENT PLAN RE-SUBMITTAL: JULY 6, 2017



**SITE LOCATION MAP**

1" = 1000'

**SITE DATA**

SUBDIVISION/DEVELOPMENT:	BUSHNELL FARMS
CITY OF FRANKLIN PROJECT NUMBER:	6471
LOT NUMBERS:	1-15, 100-105 (OPEN SPACE)
ADDRESS:	1495 & 1521 LEWISBURG PIKE
CITY:	FRANKLIN
COUNTY:	WILLIAMSON
STATE:	TENNESSEE
CIVIL DISTRICT:	10TH
EXISTING ZONING AND CHARACTER AREA OVERLAY:	GCCO-5
OTHER APPLICABLE OVERLAYS:	N/A
APPLICABLE DEVELOPMENT STANDARD:	CONVENTIONAL
ACREAGE OF SITE:	9.59
MINIMUM REQUIRED SETBACK LINES:	
BIG HOUSE HOMES	
FRONT YARD:	15 FEET
SIDE YARD:	5-10 FEET (10' ON A STREET; 10' BLDG. SEPARATION)
REAR YARD:	10' FEET
SINGLE FAMILY HOMES	
FRONT YARD:	10 FEET
SIDE YARD:	5-10 FEET (10' ON A STREET)
REAR YARD:	10 FEET
OWNERS REPRESENTATIVE:	TENN CONTRACTORS INC
ADDRESS:	P.O. BOX 314 FRANKLIN, TN 37065
PHONE NO.:	615-305-1033
CONTACT NAME:	TYLER RING
E-MAIL ADDRESS:	tyleriring@comcast.net
APPLICANT:	EDGE PLANNING, LANDSCAPE ARCHITECTURE & URBAN DESIGN
ADDRESS:	210 12TH AVENUE SOUTH, SUITE 202 - NASHVILLE TN 37203
PHONE NO.:	(615) 250-8154
FAX NO.:	(615) 250-8155
CONTACT:	JOHN HAAS - jhaas@edgela.com
ESTIMATED LANDSCAPE SURFACE RATIO:	0.10
MINIMUM LANDSCAPE SURFACE RATIO	0.10
RESIDENTIAL DENSITY	1.54 DU/AC
TREE CANOPY	41% PROVIDED (+/- 0.25 ACRES)
PARKLAND	*N/A
OPEN SPACE REQUIRED	50%
FORMAL OPEN SPACE PROVIDED	0.00 AC
INFORMAL OPEN SPACE PROVIDED	4.79 AC (50%)

**INDEX OF SHEETS**

SHEET #	TITLE
C0.0	Cover Sheet
C 1.0	Overall Existing Conditions Plan
C 2.0	Development Plan
C 3.0	Grading, Drainage & Utilities Plan
C 3.1a	Buffer Management Plan
C 4.0	Overall Access & ROW Plan
C 5.0	Overall Utility Plan
A 1.0	Architectural Elevations

PREPARED FOR:  
**TENN. CONTRACTORS INC.**  
 P.O. BOX 314  
 FRANKLIN, TN 37065  
 615.305.1033

PREPARED BY:



PROJECT TITLE  
**BUSHNELL FARM  
 PROPERTIES  
 COF NO. 6471  
 FRANKLIN, TN**

CLIENT  
**TENN  
 CONTRACTORS INC  
 FRANKLIN, TN**

PROJECT NO. 16168  
 Date 05/01/2017  
 Revisions  
 6/12/2017 SITE PLAN SUBMITTAL  
 7/6/2017 PER CITY COMMENTS

Sheet Title  
**EXISTING  
 CONDITIONS**

Sheet Number  
**C 1.00**

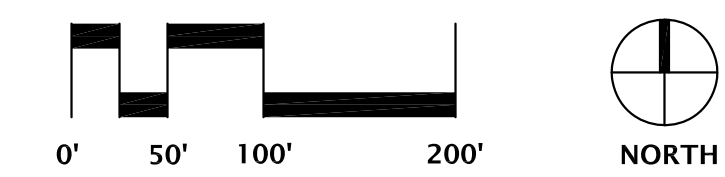


SUBDIVISION/DEVELOPMENT EXISTING ZONING	BUSHNELL FARM MGA-1 (IN PROCESS OF ANNEXATION)
MAP & PARCEL NUMBER	MAP 117, PARCELS 17.05 & 17.06
ADDRESS	1495 LEWISBURG PIKE & 1521 LEWISBURG PIKE
PROPOSED ZONING	SD-R
CHARACTER AREA	GCCO
SPECIAL AREA	5
OTHER APPLICABLE OVERLAYS	N/A
DEVELOPMENT STANDARD	CONVENTIONAL
ACREAGE OF SITE	+/- 9.59 ACRES
EXISTING TREE CANOPY	+/- 0.86 ACRES (9% OF SITE)
MIN. AREA OF EXISTING TREE CANOPY TO PRESERVE	N/A
AREA OF PRESERVED TREE CANOPY	+/- 0.31 ACRES (36% OF EXISTING CANOPY)

Soil Type	Description	Area (Acres)	Percentage
ArB	Armour silt loam, 2 to 5 percent slopes	2.9	28.2%
ArB2	Armour silt loam, 2 to 5 percent slopes, eroded	0.5	4.8%
DnB	Donerail silt loam, 2 to 5 percent slopes	1.8	18.0%
DoB2	Donerail silt loam, concretionary, 2 to 5 percent slopes, eroded	0.5	4.7%
InC3	Innen silty clay loam, 5 to 12 percent slopes, severely eroded	1.3	12.8%
MoD	Ashwood-Mimesa-Rock outcrop complex, 5 to 15 percent slopes	0.2	1.7%
SIC2	Silversville silt loam, 5 to 12 percent slopes, eroded	2.3	22.2%
W	Water	0.8	7.6%
<b>Totals for Area of Interest</b>		<b>10.3</b>	<b>100.0%</b>

**Pre-Dev  
 Grading & Drainage Data Chart**

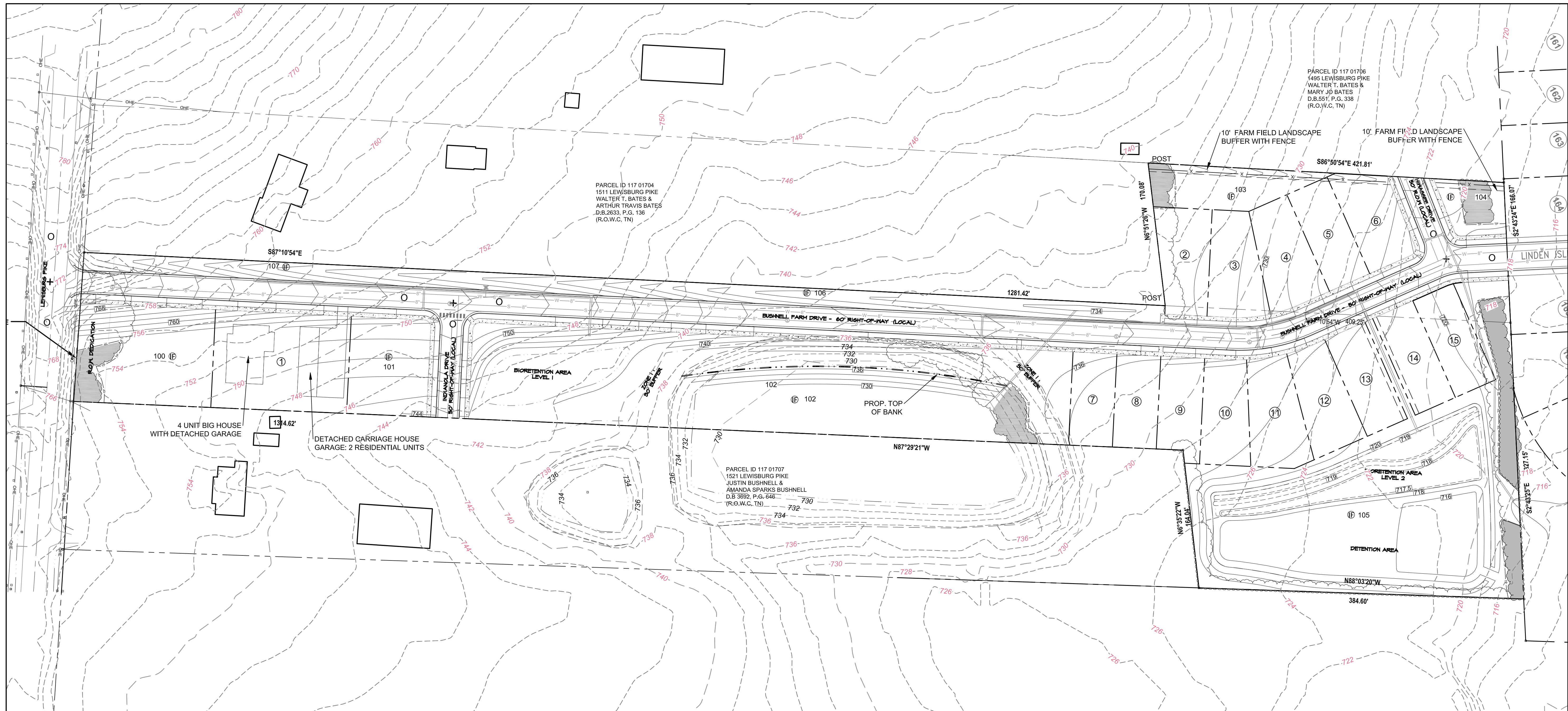
- Site Area: 26.47 AC
- Impervious Area: 0.24 AC
- Composite CN: 79
- Rv Value: 0.21
- Peak Flow Runoff
  - 2 year: 44.90 CFS
  - 5 year: 72.97 CFS
  - 10 year: 92.22 CFS
  - 25 year: 117.22 CFS
  - 50 year: 135.98 CFS
  - 100 year: 154.55 CFS



ENGINEERING PROVIDED BY:  
 MICHAEL RAY  
 ENERGY, LAND & INFRASTRUCTURE  
 1420 DONELSON PIKE, SUITE A-12  
 NASHVILLE, TN, 37217  
 (615) 383-6300  
 mray@fisherandold.com

DEVELOPER:  
 TYLER RING  
 TENN CONTRACTORS INC.  
 PO BOX 314  
 FRANKLIN, TN, 37065  
 (615) 599-0784  
 tyleriring@comcast.net

APPLICANT:  
 JOHN HAAS  
 EDGE  
 210 12TH AVE. SOUTH  
 SUITE 202  
 NASHVILLE TN, 37203  
 (615) 250-8154  
 jhaas@edgela.com



**SITE DATA**

SUBDIVISION/DEVELOPMENT EXISTING ZONING	BUSHNELL FARM MGA-1 (IN PROCESS OF ANNEXATION)
MAP & PARCEL NUMBER	MAP 117, PARCELS 17.05 & 17.06
ADDRESS	1495 LEWISBURG PIKE & 1521 LEWISBURG PIKE
PROPOSED ZONING	CONSERVATION SUBDIVISION
CHARACTER AREA	GCCO
SPECIAL AREA	5
OTHER APPLICABLE OVERLAYS	N/A
DEVELOPMENT STANDARD	CONVENTIONAL
ACREAGE OF SITE	+/- 9.59 ACRES
EXISTING TREE CANOPY	+/- 0.60 ACRES (6% OF SITE)
MIN. AREA OF EXISTING TREE CANOPY TO PRESERVE	N/A
AREA OF PRESERVED TREE CANOPY	+/- 0.31 ACRES (36% OF EXISTING CANOPY)
OPEN SPACE - REQUIRED	+/- 4.79 ACRES (50% OF GROSS DEVELOPMENT SIZE)
FORMAL OPEN SPACE - PROVIDED	NONE
INFORMAL OPEN SPACE - PROVIDED:	+/- 4.79 ACRES (50% OF GROSS DEVELOPMENT SIZE)*

**DEVELOPMENT PLAN NOTES & STATEMENT OF IMPACTS:**

- BOUNDARY INFORMATION PROVIDED BY E.L.I. TOPOGRAPHIC INFORMATION PROVIDED BY CITY OF FRANKLIN GIS DEPARTMENT.
- ALL REFUSE WILL BE COLLECTED BY THE CITY OF FRANKLIN.
- THIS PROPERTY IS REFERENCED AS: TAX MAP 117, PARCELS 17.05 & 17.06
- THERE ARE NO HISTORICAL STRUCTURES ON THIS SITE.
- THE PUD DEVELOPMENT PLAN WILL RESULT IN A TOTAL OF 20 PROPOSED UNITS, BASED ON AN AVERAGE OF 10 VEHICLE TRIPS PER DAY PER HOUSEHOLD UNIT, THIS WILL GENERATE A TOTAL OF 200 TOTAL VEHICLE TRIPS PER DAY.
- ADEQUATE TURNING MOVEMENTS SHALL BE PROVIDED FOR UTILITY AND SERVICE VEHICLES. ALL STREETS SHALL MEET THE CITY OF FRANKLIN TRANSPORTATION AND STREET TECHNICAL STANDARDS.
- INITIAL DISCUSSION WITH THE CITY OF FRANKLIN ENGINEERS INDICATES THERE IS ADEQUATE CAPACITY TO SERVE THE PROPOSED DEVELOPMENT WITH PUBLIC WATER AND SEWER SERVICES.
- ALL PUBLIC IMPROVEMENTS TO BE LOCATED WITHIN AN EASEMENT.
- THE APPLICANT WILL ENDEAVOR TO THE EXTENT POSSIBLE TO PRESERVE EXISTING SUITABLE TREES ALONG THE EDGES AND THE INTERIOR OF THE SITE. EXISTING TREES TO BE SAVED WILL BE FLAGGED PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. DURING THE CONSTRUCTION PHASE, TREE PROTECTION FENCING SHALL BE INSTALLED NEAR THE DRIP LINE OF THE PRESERVED TREES. NO CONSTRUCTION ACTIVITY OR STORAGE SHALL OCCUR WITHIN THE TREE PROTECTION ZONES.
- LIGHTINGS WILL BE PEDESTRIAN IN SCALE AND LOCATED TO ENSURE SAFE MOVEMENT OF PEDESTRIAN / VEHICLES AND FOR SECURITY PURPOSES WHILE ADHERING TO THE CITY OF FRANKLIN DESIGN STANDARDS. MIDDLE TENNESSEE ELECTRIC SHALL APPROVE STREET LIGHT STANDARDS. DIRECTIONAL LIGHTINGS WILL BE DESIGNED SO AS TO MINIMIZE GLARE AND REFLECTION ON ADJACENT PROPERTIES.
- SIGNS WILL MEET THE REQUIREMENTS OF THE CITY OF FRANKLIN ZONING ORDINANCE, CHAPTER 8.1 SIGNS. SIGN CONTROLS WILL BE ESTABLISHED FOR THE DEVELOPMENT TO SAFELY FACILITATE PEDESTRIAN AND VEHICULAR MOVEMENT IN AN ATTRACTIVE AND EFFICIENT MANNER. SIGNAGE WILL BE COMPATIBLE WITH THE SURROUNDINGS, WILL BE APPROPRIATE TO RESIDENTIAL SCALE AND EXPRESS THE IDENTITY OF THE DEVELOPMENT.
- WATER FACILITIES  
WATER SERVICE WILL BE COORDINATED WITH THE CITY OF FRANKLIN TO PROVIDE ADEQUATE FLOW AND CAPACITY. ALL WATER MAINS SHALL BE LOCATED IN A 20' PUBLIC UTILITY EASEMENT.
- SEWER FACILITIES - SEWER SERVICE WILL BE COORDINATED WITH THE CITY OF FRANKLIN TO PROVIDE ADEQUATE FLOW AND CAPACITY. ALL SEWER LINES SHALL BE LOCATED IN A 20' SANITARY SEWER EASEMENT.
- NO TRANSITIONAL FEATURES AND / OR DESIGN ELEMENTS PROPOSED ALONG THE PERIMETER OF THE PROPOSED PUD DEVELOPMENT ARE REQUIRED.
- ALL FACILITIES SHALL BE DESIGNED TO MEET ALL CITY OF FRANKLIN ORDINANCE.
- DEVELOPMENT STANDARDS WITHIN 500' OF THE SITE ARE EITHER CONVENTIONAL.
- THE BUSHNELL FARM DEVELOPMENT IS WITHIN 500' OF THE SITE

**18. WATER SEWER & RECLAIMED WATER FACILITIES**

- EXISTING FACILITIES
  - SANITARY SEWER SYSTEM  
A) UTILITY DISTRICT JURISDICTION: CITY OF FRANKLIN  
B) UTILITY DISTRICT ADDITIONAL FLOW IS 20 SFUE  
1 SFUE = 350 GALLONS/UNIT/DAY  
SFUE = SINGLE FAMILY UNIT EQUIVALENT
  - DOMESTIC WATER SUPPLY  
A) UTILITY DISTRICT JURISDICTION: CITY OF FRANKLIN  
B) DEMAND IN GALLONS PER DAY 20 X 350 GPD = 7,000 GPD  
C) DEMAND IN GALLONS PER DAY PER VISITOR: 0 GPD
  - RECLAIMED WATER  
NO RECLAIMED WATER IS REQUIRED FOR USE WITH THIS DEVELOPMENT
  - NATURAL GAS SERVICE  
A) UTILITY DISTRICT JURISDICTION: ATMOS ENERGY
  - ELECTRIC SERVICE  
A) UTILITY DISTRICT JURISDICTION: MIDDLE TENNESSEE ELECTRIC MEMBERSHIP CORPORATION
- THIS DEVELOPMENT WILL DEDICATE THE PROPOSED RIGHT-OF-WAY ALONG LEWISBURG PIKE PER THE MAJOR THORPFARE PLAN. THERE ARE NO ANTICIPATED IMPACTS ON STREETS THE BIKE PEDESTRIAN PLAN.
- FROM THIS SITE, IT IS APPROXIMATELY 5 MILES TO FIRE STATION #5 ON NOAH DRIVE AND 6 MILES TO THE CITY POLICE DEPARTMENT LOCATED ON COLUMBIA AVENUE. THE NEAREST PARK AND RECREATIONAL FACILITIES ARE LOCATED APPROXIMATELY FIVE MILES AWAY ON COLUMBIA AVENUE AT WINGSTAD HILL PARK.
- THIS DEVELOPMENT WILL RESULT IN AN INCREASE OF 20 HOUSEHOLD UNITS, BASED ON AN AVERAGE OF 0.64 SCHOOL AGE STUDENTS PER SINGLE FAMILY HOUSEHOLD, THIS WILL INCREASE THE CURRENT STUDENT SCHOOL POPULATION BY A TOTAL OF +/- 13 STUDENTS WITHIN THE FOLLOWING CATEGORIES: FASE HIGH SCHOOL, FASE MIDDLE SCHOOL, AND OAK VIEW ELEMENTARY SCHOOL.
- THE SUBJECT PROPERTY IS CONSISTENT WITH ENVISION FRANKLIN LAND-USE PLAN & THE 6000E CREEK CHARACTER AREA INCLUDING THE GUIDING PRINCIPALS OF SPECIAL AREA 5:
  - CONSERVATION SUBDIVISION DEVELOPMENT WITH ATTACHED AND DETACHED RESIDENTIAL USES
  - STREAMS, FLOODPLAIN AND TREES ARE PRESERVED AND ENHANCED AS A PUBLIC AMENITY
  - PRESERVATION OF AT LEAST 50% OF THE SITE IN DEDICATED OPEN SPACE

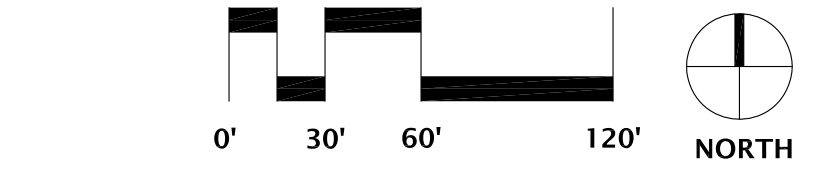
- ALL PARKING REQUIREMENTS SHALL BE MET WITH GARAGES AT EACH SINGLE-FAMILY DETACHED UNIT. BIG HOUSE UNITS SHALL USE GARAGE, STACKING AND/OR SURFACE SPACES.
- MINERAL RIGHTS ARE HELD BY TENNESSEE CONTRACTORS INC.
- NO MODIFICATION OF STANDARDS ARE BEING REQUESTED.
- NO FEE IN LIEU OF REQUESTS ARE BEING MADE.
- ALL PARKING STANDARDS FOR RESIDENTIAL AND MULTIFAMILY USES SHALL MEET THE STANDARDS WITHIN SECTION 5.9, TABLE 5-4 OF THE CITY OF FRANKLIN ZONING ORDINANCE.
- THERE IS A HISTORIC BUILDING / PROPERTY WITHIN 500' OF THE PROPOSED DEVELOPMENT.
- THERE ARE NO STRUCTURES CURRENTLY ON THE SITE.

**CONNECTIVITY INDEX**

+ : LINK  
 O : NODE  
 7 LINKS / 3 NODES = 2.33

**LEGEND**

- EXISTING TREE CANOPY
- CANOPY TO BE PRESERVED
- INFORMAL OPEN SPACE
- EXISTING CONTOURS
- PROPOSED CONTOURS
- TREE PROTECTION FENCE



**ENGINEERING PROVIDED BY:**  
 MICHAEL RAY  
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 1420 DONELSON PIKE, SUITE A-12  
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**BUSHNELL FARM  
 PROPERTIES  
 COF NO. 6471  
 FRANKLIN, TN**

**TENN  
 CONTRACTORS INC  
 FRANKLIN, TN**

PROJECT NO. 16168  
 Date 05/01/2017  
 Revisions  
 6/12/2017 SITE PLAN SUBMITTAL  
 7/6/2017 PER CITY COMMENTS

**DEVELOPMENT  
 PLAN**

Sheet Title  
 Sheet Number  
**C 2.00**

Rev.	Date	Per city comment	Revision Description
1	7/16/17		



Issue Date:	June 12, 2017
ELI Project No:	16-11-1111
Drafted By:	LCW
Checked By:	LCW

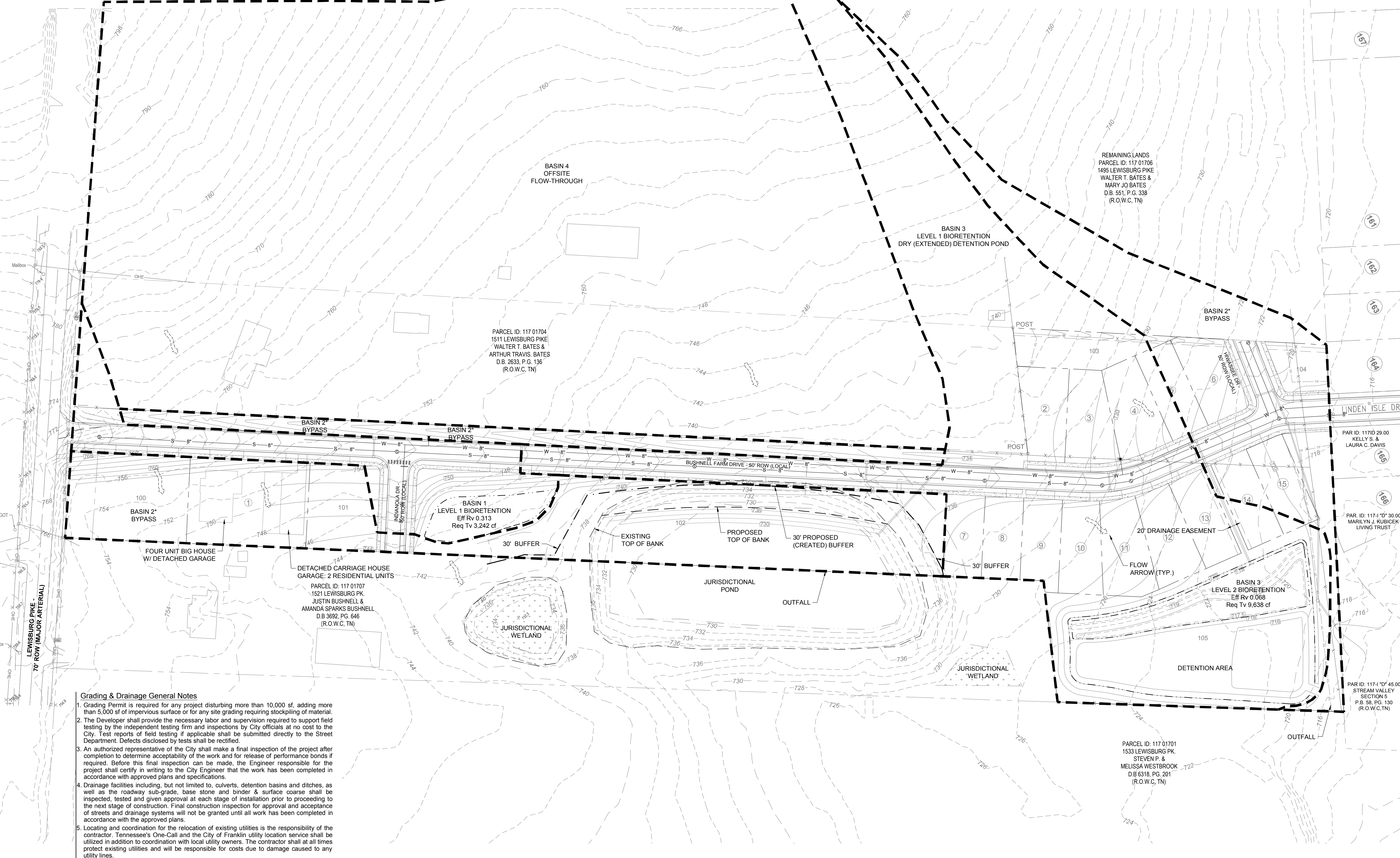
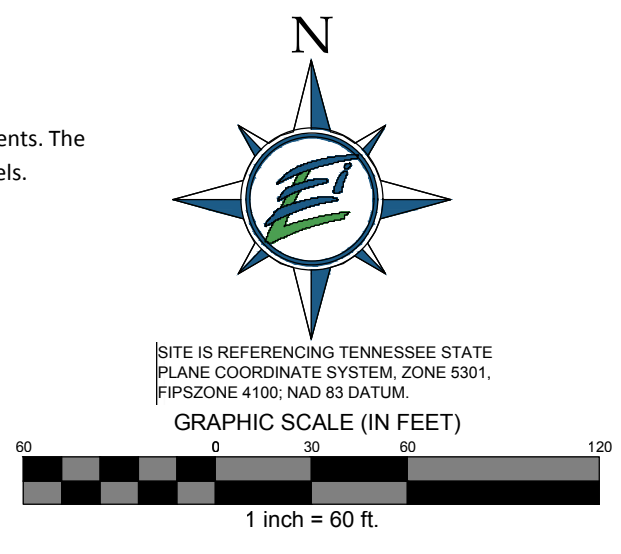
**OVERALL  
 GRADING &  
 DRAINAGE  
 PLAN**

Sheet No.  
**C 3.0**

Post-Dev Basin 1 (Bioretention 1) Grading & Drainage Data Chart	Post-Dev Basin 2 (Bypass) Grading & Drainage Data Chart	Post-Dev Basin 3 (Bioretention 2 / Extended Dry Detention) Grading & Drainage Data Chart	Post-Dev Basin 4 (Flow Through) Grading & Drainage Data Chart	Total Post-Dev Grading & Drainage Data Chart
<ul style="list-style-type: none"> <li>Site Area: 1.15 AC</li> <li>Impervious Area: 0.89 AC</li> <li>Composite CN: 94</li> <li>Disturbed Area: 1.06 AC</li> <li>Rv Value: 0.78</li> <li>Peak Flow Runoff                             <ul style="list-style-type: none"> <li>2 year: 4.87 CFS</li> <li>5 year: 6.66 CFS</li> <li>10 year: 7.83 CFS</li> <li>25 year: 9.32 CFS</li> <li>50 year: 10.41 CFS</li> <li>100 year: 11.49 CFS</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>Site Area: 5.29 AC</li> <li>Impervious Area: 0.30 AC</li> <li>Composite CN: 80</li> <li>Disturbed Area: 3.90 AC</li> <li>Rv Value: 0.24</li> <li>Peak Flow Runoff                             <ul style="list-style-type: none"> <li>2 year: 13.85 CFS</li> <li>5 year: 21.87 CFS</li> <li>10 year: 27.33 CFS</li> <li>25 year: 34.38 CFS</li> <li>50 year: 39.64 CFS</li> <li>100 year: 44.84 CFS</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>Site Area: 6.25 AC</li> <li>Impervious Area: 1.20 AC</li> <li>Composite CN: 83</li> <li>Disturbed Area: 1.57 AC</li> <li>Rv Value: 0.34</li> <li>Peak Flow Runoff                             <ul style="list-style-type: none"> <li>2 year: 18.58 CFS</li> <li>5 year: 28.38 CFS</li> <li>10 year: 34.93 CFS</li> <li>25 year: 43.32 CFS</li> <li>50 year: 49.55 CFS</li> <li>100 year: 55.68 CFS</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>Site Area: 13.78 AC</li> <li>Impervious Area: 0.31 AC</li> <li>Composite CN: 79</li> <li>Disturbed Area: 0 AC</li> <li>Rv Value: 0.22</li> <li>Peak Flow Runoff                             <ul style="list-style-type: none"> <li>2 year: 25.54 CFS</li> <li>5 year: 41.35 CFS</li> <li>10 year: 52.17 CFS</li> <li>25 year: 66.21 CFS</li> <li>50 year: 76.74 CFS</li> <li>100 year: 87.16 CFS</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>Site Area: 24.47 AC</li> <li>Impervious Area: 2.70 AC</li> <li>Composite CN: 81</li> <li>Disturbed Area: 6.53 AC</li> <li>Rv Value: 0.28</li> <li>Peak Flow Runoff                             <ul style="list-style-type: none"> <li>2 year: 39.85 CFS</li> <li>5 year: 63.29 CFS</li> <li>10 year: 82.42 CFS</li> <li>25 year: 111.80 CFS</li> <li>50 year: 132.41 CFS</li> <li>100 year: 153.06 CFS</li> </ul> </li> </ul>

**Stream Buffer Note:**  
 There shall be no mowing, clearing, grading, construction, storage, or disturbance of vegetation in riparian buffers except as permitted by the City Engineer, or his designee.

**STATEMENT OF IMPACTS**  
 City of Franklin approved BMP's will be utilized to meet or exceed water quality requirements. The post-dev runoff flows and volumes from each of the basins will not exceed the pre-dev levels.



- Grading & Drainage General Notes**
- Grading Permit is required for any project disturbing more than 10,000 sf, adding more than 5,000 sf of impervious surface or for any site grading requiring stockpiling of material.
  - The Developer shall provide the necessary labor and supervision required to support field testing by the independent testing firm and inspections by City officials at no cost to the City. Test reports of field testing if applicable shall be submitted directly to the Street Department. Defects disclosed by tests shall be rectified.
  - An authorized representative of the City shall make a final inspection of the project after completion to determine acceptability of the work and for release of performance bonds if required. Before this final inspection can be made, the Engineer responsible for the project shall certify in writing to the City Engineer that the work has been completed in accordance with approved plans and specifications.
  - Drainage facilities including, but not limited to, culverts, detention basins and ditches, as well as the roadway sub-grade, base stone and binder & surface course shall be inspected, tested and given approval at each stage of installation prior to proceeding to the next stage of construction. Final construction inspection for approval and acceptance of streets and drainage systems will not be granted until all work has been completed in accordance with the approved plans.
  - Locating and coordination for the relocation of existing utilities is the responsibility of the contractor. Tennessee's One-Call and the City of Franklin utility location service shall be utilized in addition to coordination with local utility owners. The contractor shall at all times protect existing utilities and will be responsible for costs due to damage caused to any utility lines.

Filename: W:\16-11-1111-Bushnell Property\3\_CAD\Production\Development Plan\Bushnell\_Site.dwg  
 Layout Name: Overall\_C80  
 Plot Date: 7/16/2017 12:30 pm  
 By: rpieces



**R.O.W. & Access General Notes**

1. Prior to beginning any construction, the Developer and/or Contractor, shall obtain all necessary permits as required by law. Such permits may include, but are not limited to, those required by State of Tennessee, Williamson County and other City of Franklin agencies.
2. All off-site work within the public right-of-way shall require an approved traffic control plan which complies with the MUTCD. No off-site excavation may be undertaken in any street, road, alley or right-of-way of any utility or temporary construction easement of the City of Franklin by any entity unless 72-hour notice has been given to the City of Franklin Traffic Operations Center. The contractor shall have an approved traffic control plan onsite during construction.
3. Construction materials shall be fully tested in accordance with the designations and requirements within the referenced "TDOT Standard Specifications" sections, unless otherwise noted within the "Standard Specifications" section of the City of Franklin Transportation & Street Technical Standards.
4. An authorized representative of the City shall make a final inspection of the project after completion to determine acceptability of the work and for release of performance bonds if required. Before this final inspection can be made, the Engineer responsible for the project shall certify in writing to the City Engineer that the work has been completed in accordance with approved plans and specifications.
5. Locating and coordination for the relocation of existing utilities is the responsibility of the contractor. Tennessee's One-Call and the City of Franklin utility location service shall be utilized in addition to coordination with local utility owners. The contractor shall at all times protect existing utilities and will be responsible for costs due to damage caused to any utility lines.
6. All temporary striping shall conform to "Standard Specifications for Road and Bridge Construction," published by TDOT, the latest revision except as herein amended. When approved, temporary striping shall be required prior to the opening of a street for travel where pavement and/or permanent striping cannot be completed due to weather and/or time constraints.
7. Thermoplastic pavement markings should be used on all public and private City street projects. Thermoplastic traffic striping and pavement markings shall conform to Section 716.03 "Thermoplastic Pavement Markings", of the Tennessee Department of Transportation Specifications (TDOT), and to the City of Franklin Transportation & Street Technical Standards.

**Roadway Network Narrative**

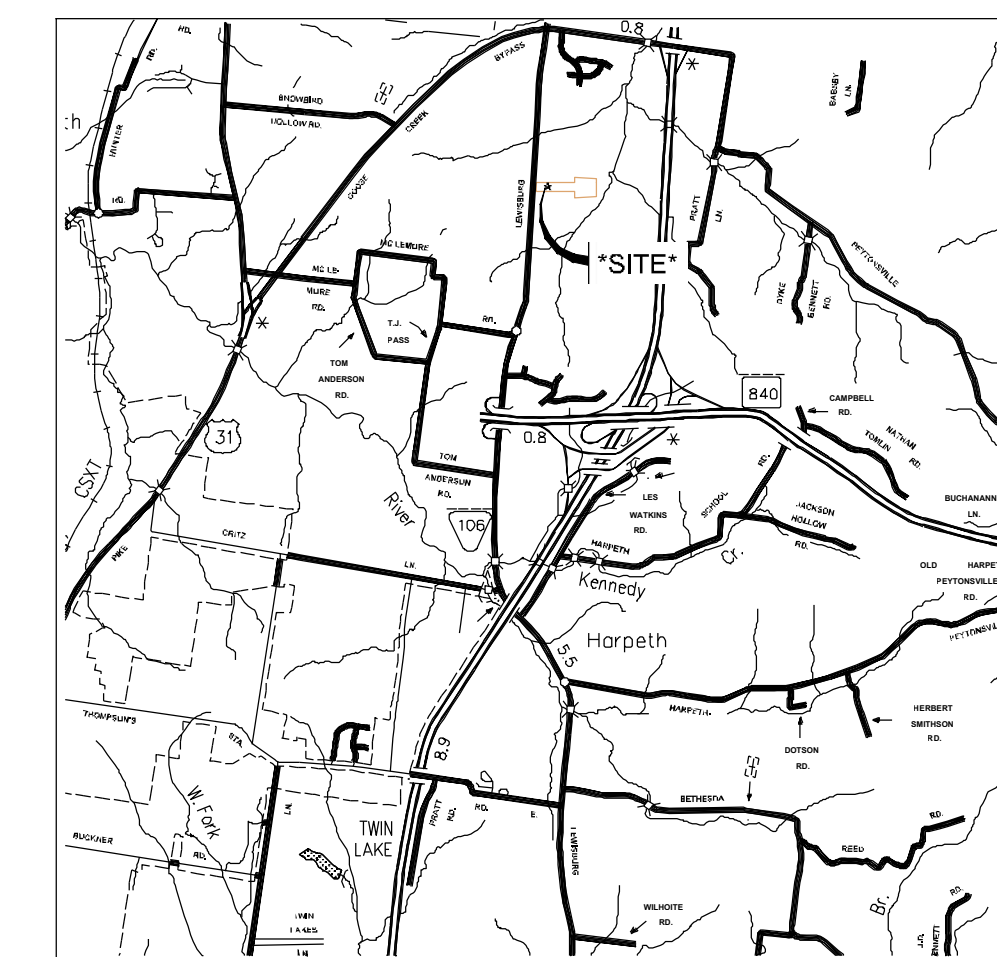
Residents will utilize the connection to Lewisburg Pike to enter and exit the site. This project will also provide an additional connection to Lewisburg Pike for the Stream Valley PUD Subdivision.

**Roadway Impact Statement**

This project is anticipated to have negligible impact on Lewisburg Pike. All Stream Valley residents currently utilize Lewisburg Pike for access. This project will add 20 total units.

**Refuse Collection & Service Areas**

Rollaway bins will be utilized for refuse collection



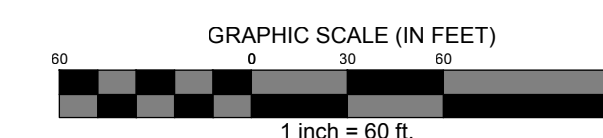
VICINITY MAP  
(NOT TO SCALE)

REMAINING LANDS  
PARCEL ID: 117 01706  
1495 LEWISBURG PIKE  
WALTER T. BATES &  
MARY JO BATES  
D.B. 551, P.G. 338  
(R.O.W.C, TN)

PARCEL ID: 117 01704  
1511 LEWISBURG PIKE  
WALTER T. BATES &  
ARTHUR TRAVIS BATES  
D.B. 2633, P.G. 136  
(R.O.W.C, TN)

PARCEL ID: 117 01707  
1521 LEWISBURG PK  
JUSTIN BUSHNELL &  
AMANDA SPARKS BUSHNELL  
D.B. 3692, PG. 646  
(R.O.W.C, TN)

PARCEL ID: 117 01701  
1533 LEWISBURG PK.  
STEVEN P. &  
MELISSA WESTBROOK  
D.B. 6318, PG. 201  
(R.O.W.C, TN)



GRAPHIC SCALE (IN FEET)  
1 inch = 60 ft.  
SITE IS REFERENCING TENNESSEE STATE  
PLANE COORDINATE SYSTEM, ZONE 5001,  
FIPSZONE 4100; NAD 83 DATUM.

**R.O.W. & Access Data Chart**

Daily Trip Generation: N/A  
Peak Hour Trip Generation: N/A  
Traffic Impact Study Required: no  
Circulation Plan Required: no

**Design Parameters**

Franklin Transportation Street Technical Standards 2014 Edition  
Design Vehicle: N/A - Private Drive  
Design Speed / Posted Speed: 20 mph / 20 mph  
Stopping Sight Distance: 115 ft  
Intersection/Driveway Sight Distance: 125 ft

**Horizontal Alignment**

Min. Centerline Radius: 70 ft  
Min. Tangent Between Curves/Intersections: 0 ft

**Vertical Alignment**

Min. / Max. Centerline Grade: 10%  
Max. Approach Centerline Grade: 5%  
Crest / Sag Curve K-Value: 7 / 17

**Access Management**

Min. Distance Between Intersection: 200 ft  
Min. Distance Between high volume/alleys & Intersections: 150 ft  
Min. Distance Between driveway & Intersections: 150 ft  
Driveway Width (two-way): 10-20 ft  
Driveway Approach Street Configuration: Curb Cut

Filename: W:\16-11-1111-Bushnell Property\3\_CAD\Production\Development\_Plan\Bushnell\_Site.dwg  
Author: Name: Overall ROW  
Date: 7/6/17  
By: rplence

**EDGE**

PLANNING ARCHITECTURE ENGINEERING DESIGN  
210 Twelfth Avenue South  
Suite 202  
Nashville, Tennessee 37203  
P 615-250-8154  
F 615-250-8155

**ENERGY LAND & INFRASTRUCTURE**  
1420 DONELSON PIKE SUITE A12 • NASHVILLE, TN 37217  
OFFICE 615-383-6300 • WWW.ELI-LLC.COM  
ENGINEERS • SURVEYORS • INFRASTRUCTURE • ENVIRONMENTAL



**Bushnell Farms**  
Development Plan  
CO# 6471

Map 117, Parcels 17.03, 17.05, & Portion of 17.06  
Franklin, Williamson County, Tennessee

Rev.	Date	Per city comment	Revision Description
1	7/6/17		



Issue Date: June 12, 2017  
ELI Project No: 16-11-1111  
Drafted By: LCW  
Checked By: LCW

Sheet Title:

**OVERALL  
ACCESS & ROW  
PLAN**

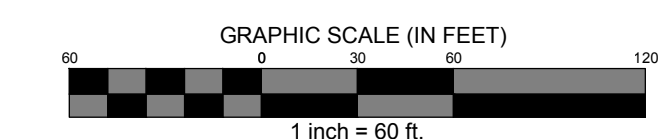
Sheet No.  
**C 4.0**

**Water & Sewer Data Chart**  
 Facility Type: Single Family & Big House Unit Residential Development  
 Unit Flow in GPD: 350 GPD (Single Family)  
 300 GPD (2 BR Big House Units)  
 250 GPD (1 BR Carriage House Units)  
 Number of Units: 14 SF Units  
 4 Big House Units  
 2 Carriage House Units  
 Total Flow in GPD: 6,600 GPD

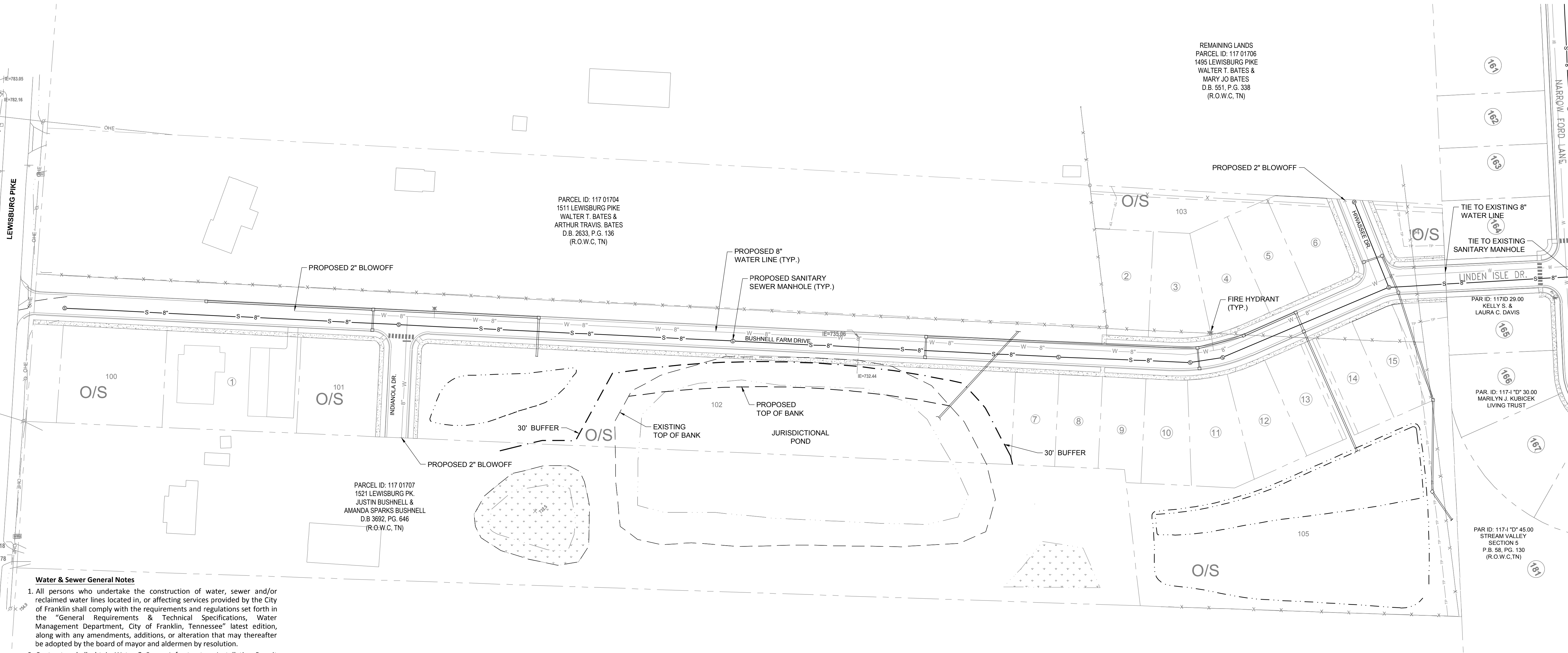
**Flow Test**  
 Date: 5/8/17 (HB&TS)  
 Hydrant No.: 977  
 Hydrant Address: 3028 Narrow Ford Lane  
 Flow Rate: 1,430 GPM  
 Hydrant No.: 976  
 Hydrant Address: 107 Crystal Falls Circle  
 Static Pressure: 120 psi  
 Residual Pressure: 110 psi

**Utility Network Narrative**  
 Water and sewer will be served from the connections to Stream Valley, PUD Subdivision, Section 5. Water and sewer stubs will be made to the two parcels to the north and water will be stubbed to the south. A fire sprinkler system shall be required in the proposed 4-unit Big House in accordance with NFPA 13R for multi-family. No sewer main is greater than 12 feet deep.

**NOTES:**  
 CITY OF FRANKLIN WATER MANAGEMENT CONTACT INFORMATION:  
 124 LUMBER DR., FRANKLIN, TN 37064, ATTN: UTILITY INSPECTOR



NOTE: THIS PROJECT IS REFERENCING TENNESSEE STATE PLANE COORDINATE SYSTEM, ZONE 5801, FIPS ZONE 4100, NAD 83 DATUM.



REMAINING LANDS  
 PARCEL ID: 117 01706  
 1455 LEWISBURG PIKE  
 WALTER T. BATES &  
 MARY JO BATES  
 D.B. 551, P.G. 338  
 (R.O.W.C. TN)

PARCEL ID: 117 01704  
 1511 LEWISBURG PIKE  
 WALTER T. BATES &  
 ARTHUR TRAVIS BATES  
 D.B. 2633, P.G. 136  
 (R.O.W.C. TN)

PARCEL ID: 117 01707  
 1521 LEWISBURG PK.  
 JUSTIN BUSHNELL &  
 AMANDA SPARKS BUSHNELL  
 D.B. 3692, PG. 646  
 (R.O.W.C. TN)

PARCEL ID: 117 01701  
 1533 LEWISBURG PK.  
 STEVEN P. &  
 MELISSA WESTBROOK  
 D.B. 6318, PG. 201  
 (R.O.W.C. TN)

**Water & Sewer General Notes**

- All persons who undertake the construction of water, sewer and/or reclaimed water lines located in, or affecting services provided by the City of Franklin shall comply with the requirements and regulations set forth in the General Requirements & Technical Specifications, Water Management Department, City of Franklin, Tennessee" latest edition, along with any amendments, additions, or alteration that may thereafter be adopted by the board of mayor and aldermen by resolution.
- Contractor shall obtain Water & Sewer Infrastructure Installation Permit prior to scheduling required Pre-Construction Site Meeting. Contractor shall complete the Pre-Construction Site Meeting prior to commencing with construction.
- Contractor shall obtain and keep all of the following items on the jobsite at all times during construction:
  - Approved, Stamped and Signed Water and/or Sewer Plans
  - Water & Sewer Infrastructure Installation Permit, with all steps completed and signed by appropriate City employees.
  - Copy of approved availability request response letter.
  - A copy of all approved Cut Sheets.
- Acceptance of City of Franklin public water infrastructure, sanitary sewer infrastructure and/or reclaimed water infrastructure shall become the responsibility of the City of Franklin, TN following approval from the City of Franklin Inspectors and acceptance by the Franklin Municipal Planning Commission. The Acceptance date shall be based on the day maintenance sureties are established by the Franklin Municipal Planning Commission.
- Prior to the acceptance of City of Franklin public water infrastructure, sanitary sewer infrastructure and/or reclaimed water infrastructure the contractor shall be required to submit as-built drawings to the Planning and Sustainability Department.
- All off-site work within the public right-of-way shall require an approved traffic control plan which complies with the MUTCD. No off-site excavation may be undertaken in any street, road, alley or right-of-way of any utility or temporary construction easement of the City of Franklin by any entity unless 72-hour notice has been given to the City of Franklin Traffic Operations Center. The contractor shall have an approved traffic control plan onsite during construction.
- Any damages caused to existing utilities during construction shall, at his own expense, be replaced or repaired to original condition and quality, as approved by the owner and representative of the appropriate utility company, by the Contractor.

**Utility Providers**

**Electric**  
 MTEMC  
 Gary Osburn  
 (615) 595-4677

**Natural Gas**  
 Atmos  
 (615) 771-8300

**Sewer**  
 City of Franklin Water & Sewer  
 Water Management Department  
 Wastewater Division  
 124 Lumber Drive  
 Franklin, TN 37064

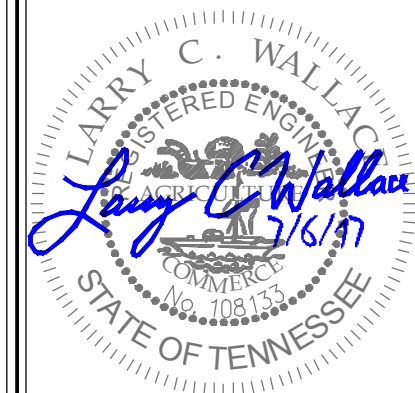
**Water**  
 HB&TS  
 Cody Lovett  
 (615) 794-7796

**EDGE**  
 PLANNING ENGINEERING ARCHITECTURE URBAN DESIGN  
 210 Twelfth Avenue South  
 Suite 202  
 Nashville, Tennessee 37203  
 P 615-250-8154  
 F 615-250-8155

**ENERGY LAND & INFRASTRUCTURE**  
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**Bushnell Farms**  
 Development Plan  
 COF # 6471  
 Map 117, Parcels 17.03, 17.05, & Portion of 17.06  
 Franklin, Williamson County, Tennessee

Rev.	Date	Per city comment	Revision Description
1	7/6/17		



Issue Date: June 12, 2017  
 ELI Project No: 16-11-1111  
 Drafted By: LCW  
 Checked By: LCW

Sheet Title:  
**OVERALL UTILITY PLAN**  
 Sheet No.  
**C 5.0**



Filename: W:\16-11-1111-Bushnell Property\3\_CAD\Production\Development Plan\Bushnell\_Site.dwg  
 Layout Name: Overall Utility  
 Author: jmurphy  
 Date: July 06, 2017 - 12:30 pm  
 By: rpierec



1 ARCHITECTURAL STYLE SAMPLES - SINGLE FAMILY HOMES WITH DETACHED REAR GARAGE  
NTS



2 ARCHITECTURAL STYLE SAMPLES - 4 UNIT BIG HOUSE  
NTS



3 ARCHITECTURAL STYLE SAMPLES - CARRIAGE HOUSE GARAGE WITH 2 RESIDENTIAL UNITS ABOVE  
NTS

**TYPICAL ARCHITECTURE NOTES:**

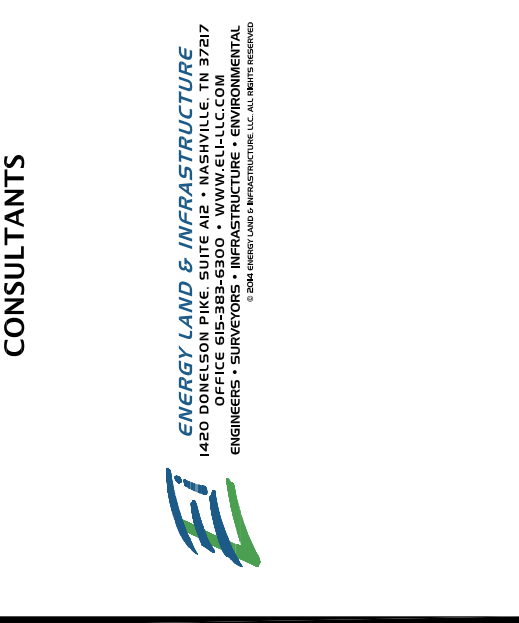
1. THESE ELEVATIONS HAVE BEEN DESIGNED TO MEET THE REQUIREMENTS OF THE CITY OF FRANKLIN'S ARCHITECTURAL DESIGN STANDARDS AND THE APPROVAL OF THE PLANNING COMMISSION / CITY OF FRANKLIN. CHANGES SHALL NOT BE MADE TO THE APPROVED ELEVATIONS UNLESS APPROVED BY EITHER THE BNS DIRECTOR OR THE PLANNING COMMISSION.
2. THESE ELEVATIONS ARE A SAMPLING OF THE PROPOSED HOME ELEVATIONS. THE APPLICANT ANTICIPATES MULTIPLE VARIATIONS OF MATERIALS OF EACH ELEVATION PROVIDED. MATERIALS AND COLORS ARE SUBJECT TO FINAL APPROVAL BY THE CITY OF FRANKLIN.
3. FINISHED FLOOR ELEVATION (FFE) SHALL BE A MINIMUM 18" ABOVE FINISH GRADE PER CITY OF FRANKLIN REQUIREMENTS.
4. PER THE CITY OF FRANKLIN ARCHITECTURAL DESIGN STANDARDS, PRIMARY FACADE MATERIALS SHALL NOT CHANGE AT OUTSIDE CORNERS AND SHALL CONTINUE ALONG ANY SIDE FACADE VISIBLE FROM A STREET RIGHT-OF-WAY, HOWEVER, MATERIALS MAY CHANGE WHERE SIDE OR REAR WINGS MEET THE MAIN BODY OF THE DWELLING.
5. FRONT LOAD GARAGE DOORS SHALL ADHERE TO STANDARDS SET FORTH IN SECTION 5.3.5(2) OF THE FRANKLIN ZONING ORDINANCE.
6. GARAGES ON CORNER LOTS WITH SIDES VISIBLE FROM A STREET RIGHT-OF-WAY SHALL INCLUDE ARCHITECTURAL DETAILS AND WINDOWS THAT MIMIC THE FEATURES OF THE LIVING PORTION OF THE BUILDING THEY SERVE.
7. ALL ARCHITECTURAL STYLES CAN BE BUILT AS FRONT LOAD, SIDE LOAD OR REAR LOAD BUILDING ELEVATIONS.
8. WHERE CORNER LOTS EXIST, NO STREET FACING FACADE SHALL HAVE A BLANK WALL.

**TYPICAL ARCHITECTURAL STANDARDS:**

PROPOSED HEIGHT OF BUILDINGS: 1-2 STORIES  
 PROPOSED SQUARE FOOTAGE OF BUILDINGS: +/- 1,800 - +/- 4,500 SF.

**ARCHITECTURAL INTENT:**

1. THE HOMES WITHIN THE DEVELOPMENT WILL BE OF AN EARLY 20TH CENTURY SOUTHERN VERNACULAR.



PROJECT TITLE  
**BUSHNELL FARM  
 PROPERTIES  
 COF NO. 6471  
 FRANKLIN, TN**

CLIENT  
**TENN  
 CONTRACTORS INC  
 FRANKLIN, TN**

PROJECT NO.	16168
Date	05/01/2017
Revisions	
△	6/12/2017 SITE PLAN SUBMITTAL
△	7/6/2017 PER CITY COMMENTS

Sheet Title  
**ARCHITECTURAL  
 ELEVATIONS**  
 Sheet Number

**A 1.00**

ENGINEERING PROVIDED BY:  
 MICHAEL RAY  
 ENERGY, LAND & INFRASTRUCTURE  
 1420 DONELSON PIKE, SUITE A-12  
 NASHVILLE, TN, 37217  
 (615) 383-6300  
 mray@fishernold.com

DEVELOPER:  
 TYLER RING  
 TENN CONTRACTORS INC.  
 PO BOX 314  
 FRANKLIN, TN, 37065  
 (615) 599-0784  
 tyleriring@comcast.net

APPLICANT:  
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 EDGE  
 210 12TH AVE. SOUTH  
 SUITE 202  
 NASHVILLE TN, 37203  
 (615) 250-8154  
 jhaas@edgela.com